



2024 Street Assessment Report

Town of Pictou – January 8, 2024

Overview

- Summary of Town Owned Streets
- Summary of Street Condition
- Budget and Performance Analysis
- Summary and Recommendations
- Questions

Summary of Town Owned Streets

- The Town owns and maintains 37.8 km of paved streets and 2 km of gravel streets
- The three classifications of streets in Pictou are as follows:
 - Local – Provides direct property access and access thru subdivisions.
 - Collector – Distributes traffic from local streets to arterials.
 - Arterial – Conveys traffic throughout one area to another with limited access to properties.

Summary of Town Owned Streets

Table 1: Classification Summary

Classification	Number of Segments	Number of km	Percent
Arterial	52	6.2	16.4%
Collector	85	9.2	24.3%
Local	188	22.4	59.3%

Condition Summary

The pavement condition index rating is based on the distress manifestation observations. The index is out of ten with lower numbers indicating better street conditions.

Table 2: Pavement Condition Index

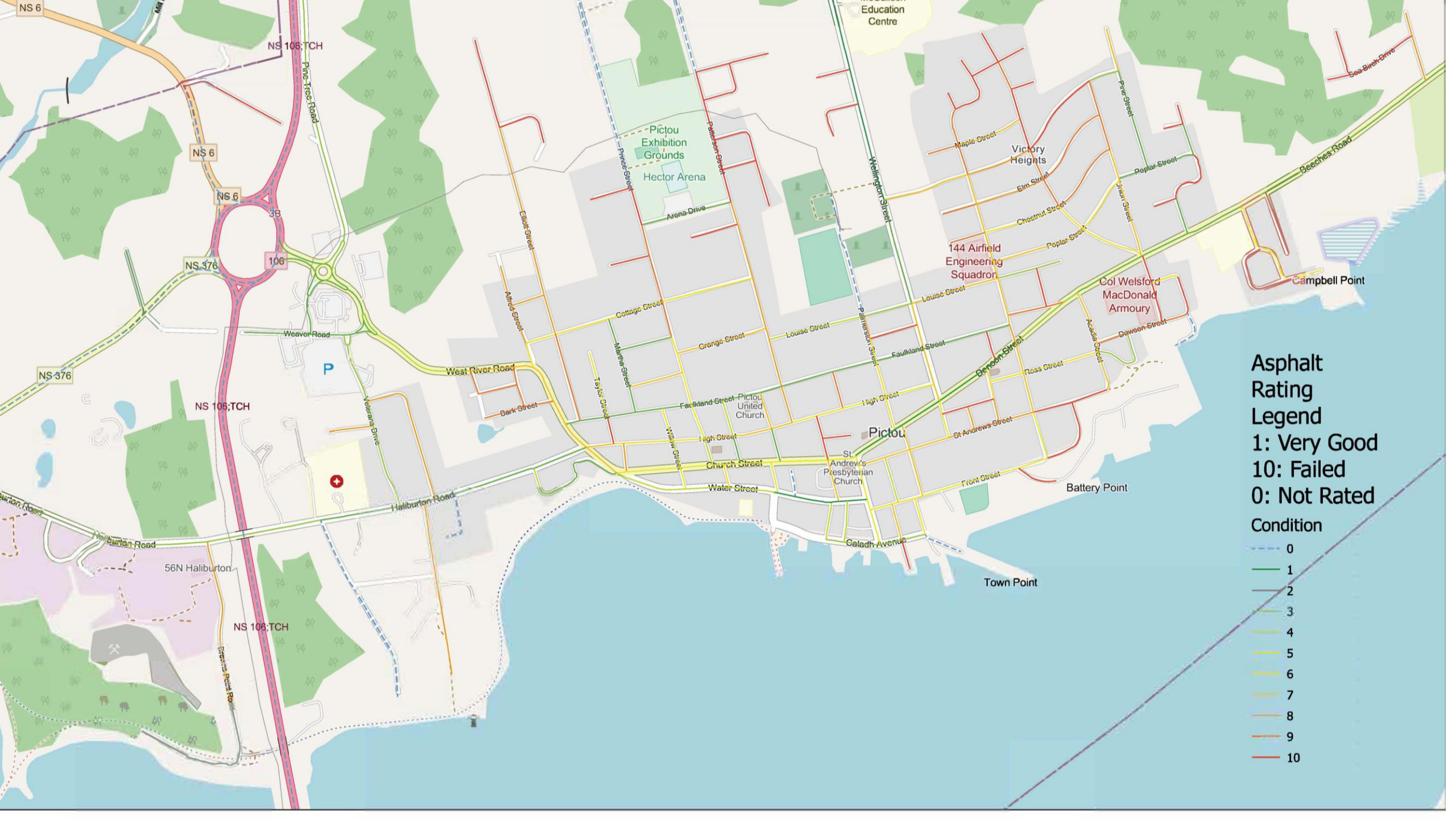
Pavement Condition Index	Pavement Condition Description
1 to 2	Very good – No to minor Maintenance required
3 to 4	Good – Basic maintenance required
5 to 6	Fair –Basic Maintenance / Resurfacing required
7 to 8	Poor – Resurfacing / Reconstruction required
9 to 10	Failed – Reconstruction Required

Condition Summary

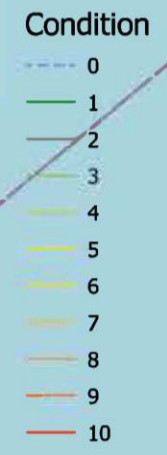
Table 3: Pavement Condition Summary

Pavement Condition Index	Pavement Condition Description	Number of Segments (325 total)	Street Length (km) (37.8 total)	Percentage of Street Network (based of km)
1 to 2	Very good – No to minor Maintenance required	74	8.87	23.5%
3 to 4	Good – Basic maintenance required	38	5.74	15.2%
5 to 6	Fair –Basic Maintenance / Resurfacing required	60	5.47	14.4%
7 to 8	Poor – Resurfacing / Reconstruction required	69	8.57	22.7%
9 to 10	Failed – Reconstruction Required	84	9.15	24.2%

Average PCI: 6



Asphalt Rating Legend
 1: Very Good
 10: Failed
 0: Not Rated



Condition Summary

- The current average PCI is 6 or fair across all street types. It should be noted that there is a bimodal distribution of the pavement condition as percents leans to the extremes.
- This is due to the recent increase in capital funding in the last 6 years and a lack of funding prior to 2016 (capital budget below \$100,000).
- There is a considerable backlog of streets with 24.2% failed and another 22.7% approaching a failed index.

Budget and Performance Analysis

The Town's capital budget should be set to address the future goals while simultaneously addressing the backlog of failed streets.

Currently the backlog is 24.7% and is expected to increase to ~40% over the next ten years at current funding levels.

The following funding scenarios will be presented:

- Existing Funding Level.
- Maintain current network average (fair) and reduced backlog.
- Maintain a network average of 4 (good).

Budget and Performance Analysis

Table 7: Budget Options

Budget Scenario	Budget year 1	Replacement percentage	Estimate average PCI at year 10	Estimated Backlog at year 10
Existing Funding	\$800,000	2.2%	8.0	40%
Maintain existing average PCI	\$1,435,000	4%	6.0	16%
Improved average	\$2,232,000	6.22%	4.0	5%

***notes:**

Existing funding assumes 5% inflation year over year

Existing funding is sourced from capital and water budgets

Maintained average and improved average funding assumes funding increases / decreases with inflation

Budget included 15% HST and 15 to 30 % contingency

Assumed 4 percent asphalt deterioration rate (3 to 5 percent typically range)

Backlog includes any streets with a rating of 9 or higher

Budget and Performance Analysis

If the capital funding for streets is not adjusted, it carries the following risks:

- Asphalt streets converting in to gravel streets
- Increased operation maintenance requirements
- Increase vehicle maintenance costs
- Reduced in street network safety
- Increase in rehabilitation costs
- Increase disruption to street users

Other Considerations

- The areas in Town with the poorest water line conditions (highest frequency of water breaks) are on West River Road, Water Street (West end) and Union Street.
- Considerations should be given to the extension of the storm water systems. The remaining trunk stormwater systems (to be installed) are located on West River Road, Union Street and Coleraine St. Once these three lines are installed, only smaller local lines will need to be constructed.
- Paving gravel streets can be considered, however paving these streets will not address the backlog of failed and poor streets.

Recommendations

A list of capital projects including costs are included in the report and can be further discussed during the capital budget discussion in February 2024. Below are a list of recommendations from the report:

- Review and update the existing sub-division bylaw and engineering specifications (2012).
- Approve projects a year in advance. This will allow for time to prepare drawings and tender packages prior to the start of the next fiscal year allowing for earlier tender posting.
- Continued focus of large projects.
- Increase funding to maintain average pavement condition index (PCI) of 6/4. This approach will improve the average PCI distribution and reduce the backlog to significantly lower levels over a 25-year period.
- Create a flexible 10-year capital plan.

Questions?

Thank You